

# Appendix 26.2: Harbourside Design Code

## Introduction

The Dunedin Harbourside has been identified as an area of significant potential for redevelopment within Dunedin City. The vision for the Harbourside area is that of a people-focused mixed-use environment, where enhanced public access to the harbour edge is the stimulus to a vibrant and thriving place to visit, work and live in.

### Purpose of Design Code

As the harbourside area redevelops, new activities will be introduced to the harbourside area, which over time will affect its look and character. This Design Code is intended as a tool to manage and influence the outcome of changes to the built form and character of harbourside, consistent with the identified character areas to create a quality environment. The intention of the Design Code is to optimise the quality of the Harbourside Zone urban environment and encourage development that will enhance the area as a vibrant people-oriented place.

The illustrations in the code are intended to support the text by explaining principles. They are not intended to represent actual design solutions.

### Relationship to District Plan

The design code forms part of the District Plan and is the basis for design assessment of controlled, discretionary (restricted or unrestricted) and non-complying activities within the Harbourside Zone. The rules establish the minimum performance standards for development while the design code will guide the qualitative outcomes.

Applicants are required to demonstrate a commitment to maintaining and extending the present and/or intended character of the Harbourside Zone. Provided that this intention is supported and the design code is followed, designers have a degree of flexibility in the preparation of development proposals.

# Structure of Design Code

This document begins with an urban character description of the harbourside area and sets out the principles of urban design upon which ongoing development is to be based. It then establishes the design criteria for new development, including design criteria specific to each identified character area. Finally, it sets out the design criteria specific to residential buildings and harbour edge public open space and wharf structures.

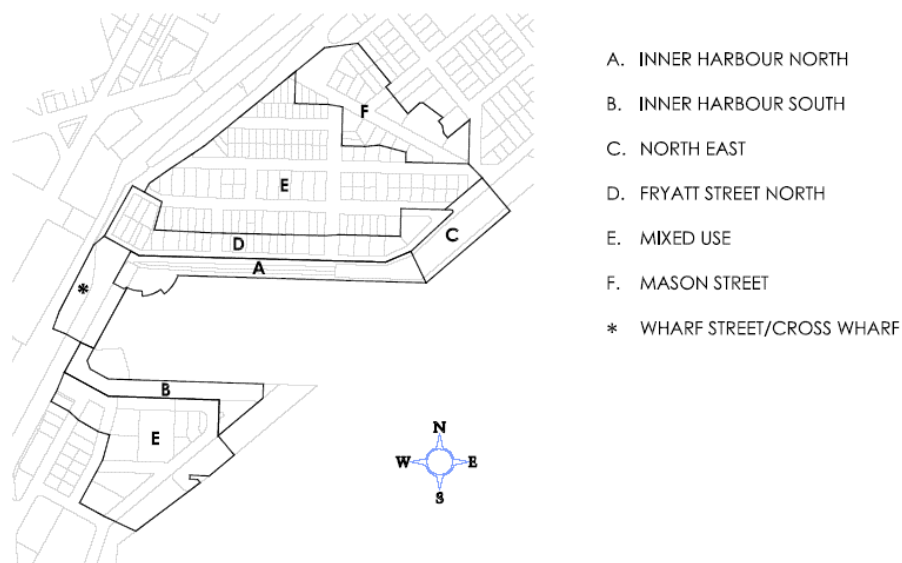
## Definition of public space

The wharf area in harbourside is currently in private ownership with limited public access based upon working wharf areas. As the area is developed for activities that move away from a working wharf area to a recreational and people oriented place, any space to which the public has generally unrestricted access in effect becomes public. These areas are marked on the Structure Plan in Appendix 26.1.1 as public open space.

It is intended that public open space forms an integral part of the urban form and as development occurs these areas will be set aside and retained as public open space. These spaces will be vested in public ownership upon subdivision.

Public space, or the public realm, refers to all areas to which the public has access – including streets and accessways, pedestrian routes, squares and wharves that are part of a private development.

## Character Area Boundaries



# Urban Character Descriptions

The opportunity for the public to access the harbour edge is a primary focus of the harbourside vision.

Central to this is the Inner Basin, traditionally the heart of the working port and the City's early gateway for both trade and new immigrants. This inlet forms the closest part of the Otago Harbour to Dunedin's city centre and this proximity offers the best opportunity for the public to access and enjoy the water's edge in the inner city. Creating and maintaining a high quality active public water edge will attract businesses, visitors and residents to the area and in so doing enhance its vibrancy and spur further development in adjoining areas.

Integral to the development of a cohesive harbour edge character is the development that occurs in the **North, South and North East Inner Basin Character Areas**.

Traditionally the hub of port activities, the wharf sheds performed an intermediary function between land and sea transport. In doing so they also formed a barrier to public access to the wharves. With the shift in function towards recreational use and redevelopment of the Inner Basin, improved public access will be encouraged by activities that facilitate public use at ground floor level, and by a series of view shafts through the wharf-side buildings creating direct public pedestrian access connecting Fryatt and Birch Streets to the water's edge.

The buildings along the Inner Basin North and Inner Basin South frame the edges to the Inner Basin and play a major role in defining its character. Building footprints for new development adhere to the narrow strip of land traditionally occupied by wharf sheds, between the wharves and the streets that run parallel to them. The design code guides the creation and enhancement of this character, and encourages a high level of public interaction between the ground floor level activities and the wharves alongside them.

It is intended that new development reflects and enhances the Port/Maritime heritage of this location, and that this character is maintained and carried through to redevelopment of the wharves.

# Urban Character Descriptions

The harbour edge is divided into a series of development segments with developers required to refurbish or rebuild the wharves to a prescribed standard concurrently with the development of the buildings alongside them in each segment. The majority of the wharf structures are located in the coastal marine area, which falls within the jurisdiction of the Otago Regional Council. To establish a cohesive urban design for the harbour edge area, it is essential that both the wharf design is integral with the design of the wharf edge buildings, and that the wharf segments collectively create a continuous and cohesive harbourside promenade where a variety of opportunities are created for public interaction with the water's edge.

The remaining character areas of harbourside include the **Fryatt Street North Character Area**, the **Mixed Use Character Area** and the **Mason Street Character Area**, all occupying the flat reclaimed areas between the Inner Basin character areas and the railway line/ Thomas Burns Street arterial route.

Cut off from the City Centre by the railway/arterial these areas have traditionally been occupied by activities that support the operations of the Port, including industry and warehouses. The street pattern here is essentially a grid, with Mason Street aligned with the street grid of the inner city and the remaining streets aligned to the Inner Basin. Street widths are a generous 21 to 25m.

Traditional industrial and port development has tended to occupy entire sites with large predominantly single-storey buildings built up to the street boundary. This has created a perimeter block layout with a continuous wall of buildings fronting up to the street edge.

As the area develops, the Harbourside Zone rules anticipate a move away from the predominant single storey port/industrial sheds to facilitate mixed use buildings of up to three, and in some cases four, stories in height. In many cases, however, the existing building fabric makes a positive contribution to the streetscape and character of the area. The design criteria of this design code encourage creative solutions that retain the key components of these buildings, amidst complimentary additions.

# Urban Character Descriptions

The **Fryatt Street North Character Area** is distinguished within harbourside by a finer grain to its built form, with buildings predominantly two storied and incorporating administrative and recreational activities. This is the result of Fryatt Street serving as the principal street facing the earliest established area of wharf and the oldest of the wharf sheds. The number of scheduled heritage buildings here reflects the distinguished tradition of the street. This prominence continues today, with identified pedestrian frontages along both sides of Fryatt Street intended to facilitate development of an active ground floor edge of retail and tourist related activities. It is intended that such premises create a ‘boutique’ scale commercial hub to harbourside, augmented by the introduction of residential and visitor accommodation at first and second floor levels.

The **Mixed Use Character Area** allows for the introduction of residential and commercial activities to the traditional industrial and service uses. Industrial features such as saw tooth roof forms and exposed brick and concrete surfaces contribute to a robust industrial character here, and new development is required to compliment this quality.

The **Mason Street Character Area** is intended to be developed with predominantly residential activities of three to four stories on either side of a central water feature spine. The water feature serves to emphasise the alignment of Mason Street to Lower Stuart Street along the city’s primary east west axis through the Octagon. The residential character is to be reinforced with a two to three metre front yard setback and development of a pedestrian priority streetscape where vehicular access is limited.

This design code encourages the use of the setbacks as private open amenity space for ground floor residential units, however screening of such spaces is regulated so as to retain a visual connection between the street and the buildings, such that the buildings remain the primary edge to the streetscape.

# Urban Character Descriptions

Improving accessibility from the inner city to the Inner Basin and pedestrian permeability within the harbourside is regarded as vital step towards creating a sustainable mixed-use environment. Mason Street is to be linked to the Inner Basin by two new pedestrian streets/walkways. The more westerly of these creates a direct link between the railway footbridge and the harbour edge, and will be a key pedestrian link from the city centre to the Inner Basin. The more easterly pedestrian street increases pedestrian connectivity through several of the longer mixed-use area blocks. These new linkages will be treated as streets with active building frontages along their edges, with vehicle access limited to residents and service vehicles.

In addition to the pedestrian walkways and the harbour edge wharf/promenade, three other significant new public spaces feature in harbourside.

A waterfront square at the junction of the Inner Basin North and the Cross Wharf forms a widening in the harbour edge promenade at the closest point to the centre of the city. The absence of contiguous harbour edge development platform here ensures good year round solar penetration. The final design of this space will provide for public gatherings, street theatre and other outdoor performance as well as varied options for the public to engage with the water's edge.

A more sheltered and intimate public square is to be formed on the south-east corner of the junction of Willis and Fish Streets. It will be framed by new development on its south and east edges, while remaining open to the north west street corner allowing for good solar penetration during the day and evening.

The third new public space occurs at the junction of Willis and Tewsley where each street corner is rebated to create a pocket for pedestrian refuge within a wider square that emphasises the Tewsley Street axis/view shaft from the Thomas Burns Street arterial to the harbour edge. This is the main vehicular access way to harbourside, and the square is given added emphasis by permitting a fourth storey to the buildings on each corner.

# Principles

Development within harbourside is based on the following key urban design principles. All applications are required to demonstrate how these principles are achieved.

## Design Coherence

A new building or public space should have its own design coherence and integrity. Rather than an ad-hoc assemblage of forms and materials to meet rules or guidelines, each development should demonstrate its own inherent design integrity and coherence that integrates and optimises all relevant design criteria.

Design coherence should not however detract from a building's relationship to its context.



*coherent corner site, mixed use*

Additions to existing buildings should be sympathetic with the expression of the original building. Where additions are made to scheduled heritage buildings the design and integrity of the original building must be respected.

However it is neither necessary nor desirable to replicate the style and appearance of heritage buildings or other existing buildings. Such techniques can undermine the integrity and authenticity of both the original building and the streetscape context. New façades can be innovative and reflect contemporary culture and technology while still relating to their context by such means as reflecting the vertical and horizontal articulation and rhythm of neighbouring buildings, or sympathetic contrast in materials or form.

The harbour edge, including the refurbished wharves and associated areas, over time, is to become a continuous public promenade. It must therefore exhibit a high level of overall design coherence and integrity in terms of architectural detail, spatial transitions and materials and fixtures, as its various segments are refurbished or rebuilt in independent stages. Particular importance is therefore placed on the first segments developed, as these will set the tone for future segments.

# Principles

## Relationship to Context

All development should consider and respect the local context, including both the local streetscape and the broader neighbourhood. The aim is to recognise the unique qualities and sense of place of the harbourside setting, and respond to and enhance these qualities in new development.

In the harbourside area, the sense of place and character is derived from a combination of the harbour edge setting, a strong historical pattern of building orientation and alignment, and robust port industrial buildings and structures. As the areas changes towards a wider



*Frvatt Street North*

mix of use, a more intimate rhythm and scale to the built form is anticipated. The challenge is to compliment and enhance the traditional character by such means as the use of similarly robust materials, reference to traditional forms or features and, where feasible, the retention of key elements of existing built fabric.

Developments in prominent locations, or accommodating activities of public significance, should consider the context of the city. Public significance is determined in terms of social and cultural relevance to the broader community of the activity. Buildings located along the harbour edge, should consider the visual prominence of the location and the impact on vantage points from across the Harbour.



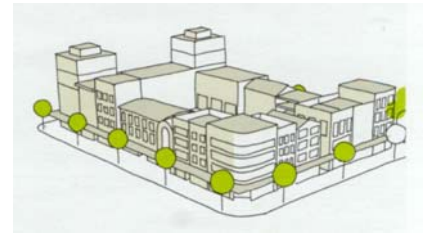
*Robust traditional character*



# Principles

## Perimeter Block Layout

The historical development pattern in the harbourside area has tended towards buildings occupying entire sites. This has created a perimeter block layout with buildings fronting the street edge, and with no side yards between buildings.



*The Block*

This has proven efficient and effective for port/industrial uses and is also most suitable for a vibrant mixed-use, pedestrian focused environment. The perimeter block layout serves to clearly define and reinforce the street and public space layout and creates an uninterrupted street frontage optimal for a pedestrian-friendly active edge condition.

To reinforce the perimeter block layout, new development should be built at full height to the edge of the street (or other public space such as a wharf, square or pedestrian lane) across the full width of the site. Large or random edge setbacks should be avoided. The



*The Core*

complex shapes or prominent location of particular sites may be recognised and expressed through a limited set back from the street edge. This must maintain the general pattern and coherence of street edge definition, and create a positive open space that demonstrably contributes to the wider system of public space.

Private and shared open amenity space is aggregated in the core of the blocks creating sheltered courtyards that provide natural light and ventilation amenity to the buildings.

Deep sites with narrow frontages are not readily adapted to perimeter block layout and where such sites exist amalgamation of titles or joint ventures incorporating neighbouring sites are recommended.

# Principles

## Active Edges

Buildings lining the edges of streets and other public spaces contribute towards the character, quality and attractiveness of the street or public space. Collectively they define the setting for the activities that take place there.



*Active identified pedestrian frontage*

Active edges are established by a strong emphasis on both visual and physical connections at the public-private interface, generally the street side façade of a building at ground floor level. Such active building edges provide a sense of occupancy and natural surveillance and contribute towards the visual interest and safety required to attract passers-

by and in doing so, enhance the vitality of the adjacent public spaces.

This principle is essential in streets with identified pedestrian frontages, where people orientated activities are encouraged. It is also important for frontages to other streets, wharves, squares, view shafts and pedestrian walkways. Where residential use occurs at ground floor level, the visual and physical connections between public and private realms needs to be more subtle in order to retain natural surveillance (eyes on the street) over the street, while attaining an acceptable level of privacy for the residents.

Distinctive entrances to buildings along a street frontage add to streetscape character and invite public interaction. Residential entrances onto the street should incorporate devices such as canopies, overhangs or recesses to create transition zone between public and private zones.

Large areas of blank wall, parking forecourts, or multiple vehicle accessways are discouraged along active edges where they inhibit pedestrian engagement and interaction.

# Principles

## Building Scale and Rhythm

As the harbourside area is redeveloped the underlying rules will over time result in a relatively high level of consistency in terms of the bulk and scale of the built environment. It is vital that a rich and varied range of expression tempers this consistency.

In the Inner Basin and North East Character Areas, the total development footprint is required to be expressed as a minimum number of contiguous, distinctly expressed, buildings.

For each of the remaining character areas within harbourside an overall maximum width of street frontage per building is recommended in the design criteria section of this code, beyond which single developments are required to express a façade as two (or more) separate architectural entities.



*Rhythm and scale on George Street*

These measures enable the retention (or formation) of a prevailing rhythm and intimacy to the streetscape without any one building becoming overly dominant, and reduce the likelihood of a single architectural gesture being overly repeated.



*vertical and horizontal articulation*

Vertical and horizontal modulation with façades allows for further articulation and expression. Careful alignment of the horizontal bands across a building can enable a new building to pick up the grain of a traditional street without mimicking the form.

For perimeter block development, the street façade is the primary element of the building as seen from the public realm. As new buildings extend to the new height limits they will in some instances stand out above their neighbours. In addition, a number of corner sites have been identified where an extra storey is permitted in order to accentuate the corners and add variation to the overall bulk and scale of the urban blocks. It is important to give attention to all façades visible from public places. Design of the roof, often considered the fifth elevation to a building, should also be considered in this respect.

# Principles

## Building Scale and Rhythm (continued)

All harbourside character areas have a prescribed maximum height as well as a prescribed maximum number of floors. The purpose of this is to provide the designer latitude for the expression of roof form, and scope to easily accommodate and conceal mechanical services. In no circumstances should additional floors be crammed into the overall maximum height permitted.

## Appropriate Parking and Servicing

As the harbourside area makes a transition from port industrial activities to a mixed-use environment there will be a change in demand for parking. Onsite parking is often inactive and unattractive and an undesirable activity at ground level where visible from public spaces. It is also undesirable within buildings along their ground level frontages to public places.

Where on-site parking is required or provided, it should not conflict with or compromise the quality of the street edge, or the status of the main pedestrian entry to the building. Parking should be located at the rear of buildings, below ground, or in some cases at first or second floor level within buildings. Under no circumstances should onsite parking be provided in parking forecourts between buildings and the street or public space. Parking should also be avoided at ground floor level along street or public space frontages within buildings.

*Nb. Semi basement parking (a half level below ground) is often very compatible with residential uses directly above in that the extra half level up creates a desirable interface between street and residence.*

The main entrances to buildings should always address the street and not parking courtyards in the centre of blocks.

Carparking is not generally regarded as an appropriate use for a heritage building, however there are a number of warehouse and industrial buildings in the harbourside that could be adapted for parking. As options for parking within Inner Basin character areas are very limited, arrangements may need to be made for meeting onsite parking needs in other nearby locations outside the Inner Basin character areas.

# Principles

## Personal Safety

Safety is an essential element of successful open spaces and can be supported by adopting the principles of Crime Prevention Through Environmental Design (CPTED).

Key components include:

- Active building edges where a sense of security is provided by way of natural passive surveillance over public space.
- A clear delineation between public spaces that are open to the public, and on-site communal spaces that are private or semi private, particularly in residential developments. The former should be open to all, the latter access-controlled to maintain safety and security. Any through site links should be designed to have a reasonable proportion of active edge.
- Effective night-time lighting with the emphasis on clearly lighting the main pedestrian routes and spaces with multiple low level light sources. This avoids the intensity and glare produced by fewer brighter light sources which can make it harder to see into darker zones of contrast further from the light source. Light spill from shopfront windows can enhance the attractiveness and safety of the street edge, when shop keepers are encouraged to keep their lights on throughout all hours of darkness.
- Avoid dark recesses or shrubs, low trees or larger objects, which offer refuge and concealment for undesirable activities.
- Provide escape routes to all publicly accessible spaces to allow exit from any potential threat.
- Avoid grilles and 'jail bars' for lock up to shops and other ground level premises. Security facilities should ideally be an unimposing and integrated part of the shopfront design.