

## 24. Dunedin International Airport

### 24.1 Introduction

Dunedin International Airport is situated at Momona. It occupies an area of approximately 300 hectares and is owned and operated by Dunedin International Airport Limited (DIAL). The airport is surrounded by rural zoned land.

Dunedin International Airport is an integral part of the local and regional transport network, providing a link to national and international destinations for passengers, goods and freight. There has been steady growth in passenger numbers and freight volumes, both domestic and international, and a continuation of this growth is expected. The airport is a physical resource that provides for the economic and social well-being of the Dunedin area and beyond.

The operation of the airport is undertaken in accordance with two designations (Aerodrome and Runway Extension, and Flight Path Protection), which extend across the majority of the Dunedin International Airport Zone and beyond. The designations focus upon ensuring the safe and efficient operation of the airport activities, while securing a future runway extension and providing a limited range of associated airport activities.

The zone includes the airport and immediately adjacent rural land, together with the site occupied by Momona Garage. Momona village is zoned residential. The Dunedin International Airport Zone recognises the function of the Dunedin International Airport within the transport network, sustainable management of the resource and the need to manage effects of activities located at the airport. The zone enables an integrated approach to the management of the airport resource.

The intention is that development within the Dunedin International Airport Zone is clearly associated with the operation and functions of the airport. This will ensure land within the zone is retained for its primary operational functions. The provisions allow for the continued use and development of Dunedin International Airport and provide certainty for airport operators and the community, while managing effects on neighbouring land uses.

## 24.2 Objectives and Policies

| <b>Objective 24.2.1</b>   |  |
|---|--|
| Dunedin International Airport is able to operate efficiently and effectively. |  |
| Policy 24.2.1.1   | Enable airport activity at Dunedin International Airport.  |
| Policy 24.2.1.2   | Provide for commercial activities ancillary to airport activity, office activity, service stations and visitor accommodation where they are designed and operated in line with Objective 24.2.2 and its policies.  |
| Policy 24.2.1.3   | Only allow activities that are not ancillary to airport activity where: <ol style="list-style-type: none"> <li>they are related to or support Dunedin International Airport, or have other operational requirements that mean they need to locate in the zone;</li> <li>they will support the efficient and effective operation of Dunedin International Airport;</li> <li>they are not more appropriately located in another zone in line with Objective 2.3.2 and its policies; and</li> <li>they are designed and operated in line with Objective 24.2.2 and its policies.</li> </ol> |
| Policy 24.2.1.4   | Enable land that is surplus to the needs of Dunedin International Airport to transition to the Taieri Plains Rural Zone, so that future development that is not related to airport activity can be managed in accordance with the objectives and policies of that zone.  |
| Policy 24.2.1.5   | Only allow subdivision where: <ol style="list-style-type: none"> <li>it is in accordance with the provisions of the rural zones; and</li> <li>the subdivision does not adversely affect the efficient and effective operation of Dunedin International Airport.</li> </ol>   |

| <b>Objective 24.2.2</b>  |  |
|--|--|
| Land use activities and development necessary to meet the reasonably foreseeable needs of Dunedin International Airport is enabled, while ensuring it: <ol style="list-style-type: none"> <li>achieves a high standard of on-site amenity for airport users; and</li> <li>minimises adverse effects on rural amenity as far as practicable.</li> </ol> |  |
| Policy 24.2.2.1  | Require buildings and structures to be of a height and setback that reduces their visual impact on the rural environment.  |
| Policy 24.2.2.2  | Require landscaping of new buildings, parking areas and outdoor storage areas, to ensure a high standard of visual amenity is maintained adjacent to public roads and the rural zones.   |
| Policy 24.2.2.3  | Require development to maintain on-site and rural amenity by ensuring service areas are not visible from publicly accessible areas within or outside the zone.   |
| Policy 24.2.2.4  | Require ancillary signs to be located and designed to maintain visual amenity for airport users, including by being of an appropriate size and number to convey information about the name, location and nature of the activities on-site, and not being too numerous or oversized for that purpose. |
| Policy 24.2.2.5  | Require commercial activity ancillary to airport activity to be designed, located and operated to primarily serve staff and visitors to the Dunedin International Airport.   |

### Objective 24.2.2

|                 |  |
|-----------------|--|
| Policy 24.2.2.6 | Enable tourism advertising signage promoting Otago and Southland's tourism industry in order to facilitate the economic and social prosperity of the two regions, while ensuring the size, number and location of tourism advertising signage does not significantly affect the amenity of the airport as an international gateway, and any effects on the rural amenity outside the Dunedin International Airport Zone are minor. |
|-----------------|--|

### Objective 24.2.3

Earthworks necessary for permitted or approved land use and development are enabled, while avoiding, or adequately mitigating, any adverse effects on:

- a. visual amenity and character;
- b. the stability of land, buildings, and structures; and
- c. surrounding properties.

|                 |   |
|-----------------|---|
| Policy 24.2.3.1 | Require earthworks, and associated retaining structures, to be designed and located to avoid adverse effects on the stability of land, buildings, and structures by: <ol style="list-style-type: none"> <li>a. being set back an adequate distance from property boundaries, buildings, structures and cliffs; and</li> <li>b. using a batter gradient that will be stable over time.</li> </ol>  |
| Policy 24.2.3.2 | Require earthworks and any associated retaining structures to be designed and located to minimise adverse effects on surrounding sites and the wider area, including by: <ol style="list-style-type: none"> <li>a. limiting the scale of earthworks that are provided for as a permitted activity; and</li> <li>b. requiring earthworks to avoid sediment run-off, including onto any property, or into any stormwater pipes, drains, channels or soakage systems.</li> </ol>   |
| Policy 24.2.3.3 | Only allow earthworks that exceed the scale thresholds (earthworks - large scale) and any associated retaining structures, where all of the following effects will be avoided or, if avoidance is not possible, adequately mitigated: <ol style="list-style-type: none"> <li>a. adverse effects on visual amenity and character;</li> <li>b. adverse effects on the amenity of surrounding properties, including from changes to drainage patterns; and</li> <li>c. adverse effects on the stability of land, buildings, and structures.</li> </ol> |

## Rules

### Rule 24.3 Activity Status

#### 24.3.1 Rule location

The activity status tables in Rules 24.3.3 to 24.3.6 specify the activity status of land use activities, development activities, and subdivision activities in the Dunedin International Airport Zone and relevant overlay zones, except rules for the following city-wide activities, which are contained in Part B of the Plan:

1. Public Amenities (Section 3)
2. Temporary Activities (Section 4)
3. Network Utilities and Energy Generation (Section 5)
4. Transportation Activities (Section 6)
5. Scheduled Trees (Section 7)
6. Natural Hazard Mitigation (Section 8)

#### 24.3.2 Activity status introduction

1. The activity status tables in Rules 24.3.3 - 24.3.6 show the activity status of activities in the Dunedin International Airport Zone and the overlay zones indicated, provided any performance standards shown in the far right column are met.
2. Activities are split into three types: a. land use activities, b. development activities, c. subdivision activities. Within these types, activities are split into categories. The nested tables in Section 1.6 outline the hierarchy of categories, activities and sub-activities.
3. Permitted land use activities do not automatically provide for any related development activities. Therefore, the activity status of any associated development activities should also be checked.
4. Where a new land use activity requires a resource consent, all associated development activities will be considered and assessed as part of the resource consent even if the development otherwise meets the development performance standards listed in the Plan.
5. The nested table in Section 1.6 is intended to be a complete list of activities and categories of activities. For any activity that is not covered by any of the defined activities in the nested table, the activity status will be non-complying if the activity status of "all" or "all other" activities in the most closely related category is non-complying, otherwise it will be discretionary.

#### *Additional activity status rules in hazard overlay zones*

6. For the purpose of the hazards provisions, activities are categorised as sensitive activities, potentially sensitive activities or least sensitive activities. The activities that are in each hazards sensitivity category are included in the definitions section and in Section 11.1.
7. In the hazard 2 overlay zones, the activity statuses in Rule 24.3.6 apply to the following activities:
  - i. new sensitive activities and potentially sensitive activities, and
  - ii. some new buildings.
8. Where the activity status in Rule 24.3.6 differs from that in Rule 24.3.3 - 24.3.5, the most restrictive activity status always applies.
9. In addition to the rules in Rule 24.3.6, performance standards for development activities within hazard overlay zones are included in Rule 24.3.3 - 24.3.5.

10. Activities in a hazard overlay zone must comply with all of the rules in 24.3.3 - 24.3.6.

### Performance Standards

11. Performance standards are listed in the far right column of the activity status tables.
12. Performance standards apply to permitted, controlled, and restricted discretionary activities.
13. If a permitted or controlled activity does not meet one or more performance standards, then the activity status of the activity will become restricted discretionary, unless otherwise indicated by the relevant performance standard rule.
14. If a restricted discretionary activity does not meet one or more performance standards, then the activity status remains restricted discretionary, unless otherwise indicated in the performance standard.

### Legend

| Acronym | Activity status   |
|---------|---|
| —       | No change to activity status or not relevant (intersect does not exist) |
| P       | Permitted activity  |
| C       | Controlled activity   |
| RD      | Restricted Discretionary activity                                       |
| D       | Discretionary activity  |
| NC      | Non-complying activity  |
| Pr      | Prohibited  |
| Acronym | Zone/overlay zone name  |
| DIA     | Dunedin International Airport Zone                                      |
| ANIC    | Airport Noise Inner Control mapped area                                 |
| Haz2    | Hazard 2 Overlay Zones  |

### 24.3.3 Activity status table - land use activities

|                           |  |                 |         |   |
|---------------------------|--|-----------------|---------|---|
| 1.                        | Performance standards that apply to all land use activities    |                 |         | i. Acoustic insulation (noise sensitive activities)<br>ii. Electrical interference<br>iii. Light spill<br>iv. Noise |
| Major facility activities |  | Activity status |         | Performance standards   |
|                           |  | a. DIA          | b. ANIC |   |
| 2.                        | Airport  | P               | —       | i. Minimum car parking  |
| 3.                        | Emergency services   | P               | —       |   |
| 4.                        | All other activities in the major facility activities category | NC              | —       |   |

| Commercial activities  |  | a. DIA | b. ANIC | Performance standards                                |
|------------------------|--|--------|---------|--|
| 5.                     | Commercial activities <i>ancillary to airport activity</i>                 | P      | —       | i. Location  |
| 6.                     | Conference, meeting and function <i>ancillary to visitor accommodation</i> | P      | —       |  |
| 7.                     | Office   | D      | —       |  |
| 8.                     | Service stations   | RD     | —       | i. Service station performance standards             |
| 9.                     | Visitor accommodation  | P      | —       | a. Minimum car parking<br>b. Minimum vehicle loading |
| 10.                    | Tourism advertising  | P      | —       | a. Number and location of tourism signs              |
| 11.                    | All other activities in the commercial activities category                 | NC     | —       |  |
| Residential activities |  | a. DIA | b. ANIC | Performance standards                                |
| 12.                    | Standard residential   | NC     | Pr      |  |
| 13.                    | Working from home  | P      | Pr      |  |
| 14.                    | Supported living facilities  | NC     | Pr      |  |
| Community activities   |  | a. DIA | b. ANIC | Performance standards                                |
| 15.                    | Conservation   | P      | —       |  |
| 16.                    | All other activities in the community activities category                  | NC     | —       |  |
| Rural activities       |  | a. DIA | b. ANIC | Performance standards                                |
| 17.                    | Farming  | P      | —       |  |
| 18.                    | Grazing  | P      | —       |  |
| 19.                    | All other activities in the rural activities category                      | NC     | —       |  |
| Industrial activities  |  | a. DIA | b. ANIC | Performance standards                                |
| 20.                    | All activities in the industrial activities category                       | D      | —       |  |

#### 24.3.4 Activity status table - development activities

| Building and structures activities |   | Activity status | Performance standards  |
|------------------------------------|---|-----------------|--|
| 1.                                 | All buildings and structures activities | P               | a. Boundary setbacks<br>b. Fire fighting<br>c. Maximum height<br>d. Minimum floor level<br>e. Number, location and design of ancillary signs |

| Site development activities |   | Activity status | Performance standards  |
|-----------------------------|---|-----------------|--|
| 2.                          | Earthworks - small scale                | P               | a. Earthworks standards  |
| 3.                          | Earthworks - large scale                | RD              | a. Earthworks standards  |
| 4.                          | Outdoor storage                         | P               | a. Boundary treatments and other landscaping   |
| 5.                          | Service areas                           | P               | a. Location and screening of service areas   |
| 6.                          | Parking, loading and access             | P               | a. Boundary treatments and other landscaping<br>b. Parking, loading and access standards |
| 7.                          | Storage and use of hazardous substances | P               | a. Hazardous substances quantity limits and storage requirements                         |
| 8.                          | All other site development activities   | P               |  |

#### 24.3.5 Activity status table - subdivision activities

| Subdivision activities |                        | Activity status | Performance standards                |
|------------------------|------------------------|-----------------|--------------------------------------|
| 1.                     | Subdivision activities | RD              | a. Subdivision performance standards |

#### 24.3.6 Change to activity status in Hazard 2 Overlay Zones

| Activity |   | Haz2 |
|----------|---|------|
| 1.       | Potentially sensitive activities not permitted in Dunedin International Airport Zone  | D    |
| 2.       | Sensitive activities  | D    |
| 3.       | New buildings, and additions and alterations to buildings, which create more than 60m <sup>2</sup> of new ground floor area | RD   |

#### Note 24.3A - Guidance on existing use rights applying to land use activities in hazard overlay zones

1. For the purposes of the natural hazards provisions only, with respect to section 10 of the RMA, Council will generally consider that a land use activity is similar in character, intensity, and scale where:
  - a. for a residential activity, there is less than 25m<sup>2</sup> increase in ground floor area of any residential building(s), in any consecutive 10 year period; or
  - b. for a residential activity, a new building is to be used solely as a garage or shed; or
  - c. for all other sensitive activities and potentially sensitive activities, the ground floor area of any buildings increases by less than 100% in any consecutive 10 year period.
2. However, Council will consider specific circumstances associated with the development and how this affects the character, intensity and scale of the land use activity.

**Note 24.3B - General advice**

1. Under the Heritage New Zealand Pouhere Taonga Act 2014 it is an offence to modify or destroy an archaeological site without obtaining an archaeological authority from Heritage New Zealand (HNZ). This is the case regardless of whether the land on which the site is located is designated, or the activity is permitted under the District Plan or Regional Plan, or a resource or building consent has been granted.
2. An archaeological site is defined as any place in New Zealand that was associated with human activity that occurred before 1900 and provides or may provide, through investigation by archaeological methods, evidence relating to the history of New Zealand. This includes the scheduled archaeological sites within the District Plan.
3. Pre-1900 buildings are considered archaeological sites. An archaeological authority must be obtained before demolishing a pre-1900 building or structure.
4. The **archaeological alert layer mapped area** identifies sites and areas that have a strong likelihood of containing archaeological remains, however remains may be present anywhere within the District. If archaeological remains are discovered during any site works, at any location, the Accidental Discovery Protocol (Appendix A8) should be followed.
5. All buildings and structures activities associated with existing major facilities are advised to undertake an assessment of risk from natural hazards and implement any recommended risk mitigation measures.

**Note 24.3C - General advice**

1. Dunedin International Airport Limited holds two designations, D273 and D274, for airport-related activity and structures.
2. Airways Corporation of New Zealand Limited hold a number of designations for airport-related structures.
3. Details of the designations can be found in Appendix A1.4.

**Rule 24.4 Notification**

1. Activities are subject to the normal tests for notification in accordance with sections 95A-95G of the RMA.



## Rule 24.5 Land Use Performance Standards

### 24.5.1 Acoustic Insulation

Noise sensitive activities in the **airport noise outer control mapped area** and the **airport noise inner control mapped area** must comply with Rule 9.3.1.

### 24.5.2 Electrical Interference

Land use activities must comply with Rule 9.3.2.

### 24.5.3 Light Spill

1. On-site lighting (other than runway, navigational and other operational lighting) within the Dunedin International Airport Zone must be positioned, directed and maintained so that light spill outside of the zone does not exceed 16 Lux when measured:
  - a. 10 metres outside the zone boundary;
  - b. in both the horizontal and vertical planes; and
  - c. with the runway, navigation and associated operational lighting turned off.
2. Rule 24.6.3.1 does not apply where the adjoining zone is the residential zone.
3. Light spill measured at the boundary of the residential zone or any site used for residential purposes or zoned residential must not exceed the following limits:

| Time |            | Limit  |
|------|------------|--------|
| a.   | 7am- 10pm  | 10 Lux |
| b.   | 10pm - 7am | 3 Lux  |

- a. except, this standard does not apply to headlights of motor vehicles, operating on roads or driveways/vehicle tracks, but it does apply to parking/loading areas with over 5 vehicles.
  - b. any outdoor lighting must ensure that direct or indirect illumination does not exceed 3 lux at the windows of residential buildings within residential zones.
4. Activities that contravene the performance standard for light spill are non-complying activities.

### 24.5.4 Location

For office activity and commercial activities ancillary to airport activity (except vehicle rental facilities), customer access must only be available from inside terminal buildings.

### 24.5.5 Minimum Car Parking

1. Airport activity must provide a minimum of 971 parking spaces, including 20 mobility parking spaces.
2. Visitor accommodation must provide:
  - a. 1 parking space per 3 guest rooms, where the activity is based upon guest rooms (e.g. hotels);
  - b. 1 parking space per visitor accommodation unit, where the activity is based on units (e.g. motels); and
3. Visitor accommodation must provide mobility parking spaces as follows:

| Total number of parking spaces provided |        | Minimum number of these that must be mobility parking spaces |
|---|--------|--|
| i.                                      | 1 - 20 | 1 parking space  |

| Total number of parking spaces provided |  | Minimum number of these that must be mobility parking spaces |
|---|--|--|
| ii.                                     | 21 - 50                                | 2 parking spaces   |
| iii.                                    | For every additional 50 parking spaces | 1 additional parking space                                   |

4. Required parking spaces may be used for car, cycle or motorcycle parking, except for any required mobility parking spaces, which must be used for mobility car parking.
5. Where the minimum car parking performance standard results in the requirement for a fractional space, any fraction under one half will be disregarded and any fraction of one half or greater will be counted as one space.

**Note 24.5A - Other relevant District Plan provisions**

1. Parking areas must comply with the standards in Section 6.6: Parking, Loading and Access Performance Standards.

**24.5.6 Minimum Vehicle Loading**

1. Visitor accommodation units with greater than 50 individual rooms must provide one loading space, to accommodate a coach with minimum dimensions as shown in Rule 6.14 as Figure 6.14L.
2. Required vehicle loading must be designed and constructed to comply with Rule 6.6.2.

**24.5.7 Noise**

Land use activities must comply with Rule 9.3.6.

**24.5.8 Service Station Performance Standards**

Service stations must comply with Rule 6.7.1.

**24.5.9 Number and Location of Tourism Signs**

Tourism advertising located outside terminal buildings must not:

1. exceed a total maximum signage area of 200m<sup>2</sup>;
2. be visible from a public place outside the Dunedin International Airport Zone;
3. exceed 8m in height;
4. exceed 400 m in depth;
5. have any display face that exceeds 18 m<sup>2</sup>; or
6. obstruct parking, loading or access areas.

## Rule 24.6 Development Performance Standards

### 24.6.1 Boundary Setbacks

1. New buildings and structures, and additions and alterations, must be set back 20m from any boundary with a rural zone or any road.
2. Except fences, ancillary signs and all buildings and structures in section 73 IV Maungatua Survey District (CT 15D/487) are exempt from the performance standard.

### 24.6.2 Boundary Treatments and Other Landscaping

1. For all parking areas, new buildings and outdoor storage areas within 5m of Miller Road, Otokia Road, Centre Road or a rural zone, a landscaping area with a minimum width of 1.5m must be provided along the full length of the road frontage (except for where vehicle access is provided).
2. Landscaping areas must:
  - a. be planted with a mix of trees and shrubs and/or ground cover plants that achieves a total coverage of the ground area in planting (when mature), except for 10% of the area, which may be used for pedestrian paths;
  - b. have an average of one tree for every 5m of frontage;
  - c. not have more than 10% cover in impermeable surfaces (for pedestrian paths);
  - d. be designed to allow surface water run-off from surrounding areas to enter;
  - e. be protected by a physical barrier that prevents cars from accidentally driving into or damaging plants;
  - f. for required trees, use trees that are at least 1.5m high at the time of planting and capable of growing to a height of 5m within 10 years of planting;
  - g. be planted prior to occupation or completion of any relevant building(s) or site development; and
  - h. be maintained to a high standard, which means trees and under-planting are healthy and areas are regularly cleared of rubbish and weeds.
3. Any road boundary fences provided must be placed on the property side of any required road frontage landscaping.
4. For any parking areas greater than 200m<sup>2</sup> (excluding loading areas) a minimum of 1m<sup>2</sup> of additional landscaped area must be provided for every parking space, either within or adjoining the parking area.

### 24.6.3 Earthworks Standards

#### 24.6.3.1 Earthworks - small scale thresholds

- a. Earthworks must not exceed the following scale thresholds to be considered earthworks - small scale. Where earthworks are located in the Haz2 overlay zone, the most restrictive scale threshold applies for the purposes of determining activity status. Resource consents will be assessed against all scale thresholds that are contravened.

| Zone/Area |   | 1. Dunedin International Airport Zone          | 2. Haz2 (Flood)       |
|-----------|---|--|-----------------------|
| i.        | Maximum change in ground level          | 2.0m   | —                     |
| ii.       | Maximum volume of combined cut and fill | 30m <sup>3</sup> per 100m <sup>2</sup> of site | 20m <sup>3</sup> fill |

- b. Earthworks *ancillary to network utilities* activities are only required to comply with Rule 24.6.3.1.a.i - maximum change in ground level threshold.
- c. Earthworks *ancillary to the operation, repair, and maintenance of the roading network* are exempt from the

performance standard earthworks - small scale thresholds.

- d. Scale thresholds will be calculated as the cumulative total of earthworks on any site in a two calendar-year period.
- e. Earthworks that exceed the earthworks - small scale thresholds are treated as earthworks - large scale, which are a restricted discretionary activity.

#### 24.6.3.2 Batter gradient

Earthworks must:

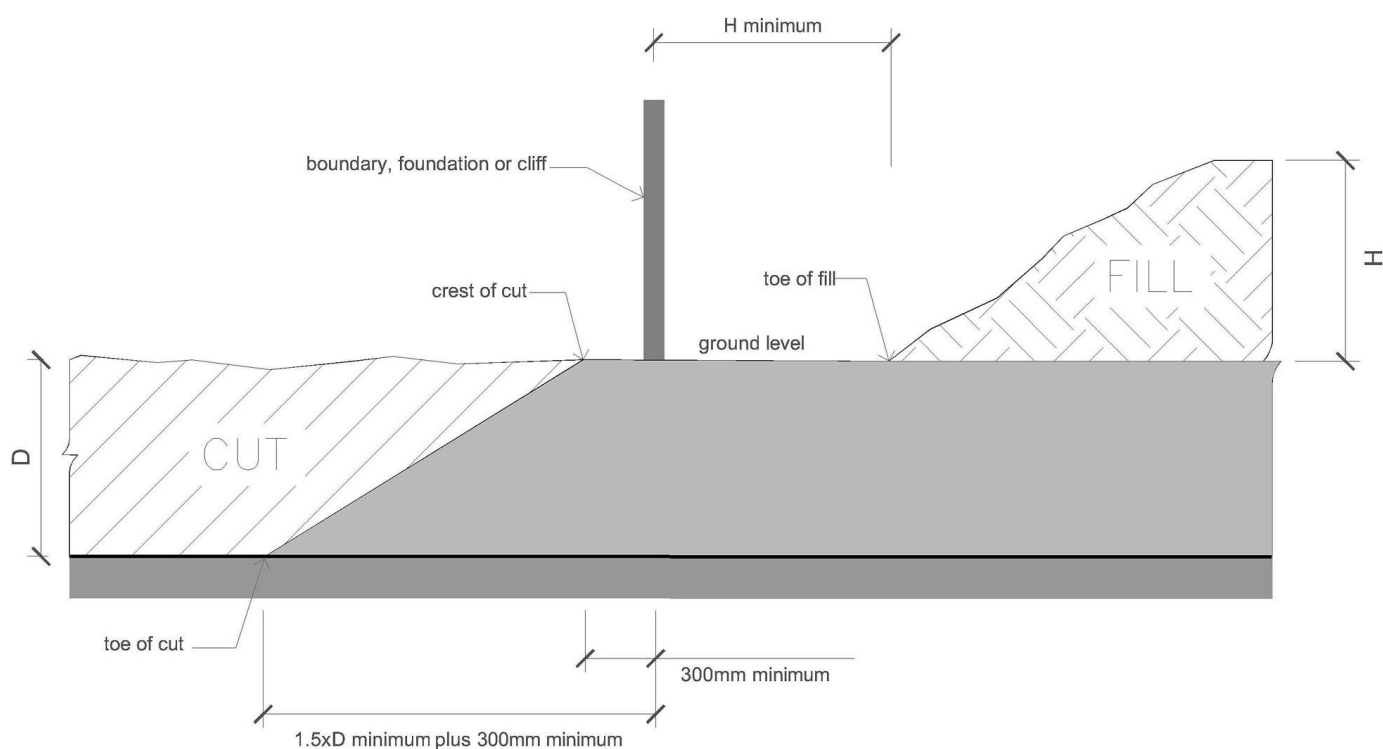
- a. have a maximum cut batter gradient of 1:1 (i.e. rising 1m over a 1m distance); and
- b. have a maximum fill batter gradient of 2:1 (i.e. rising 1m over a 2m distance).

#### 24.6.3.3 Setback from property boundary, buildings, structures and cliffs

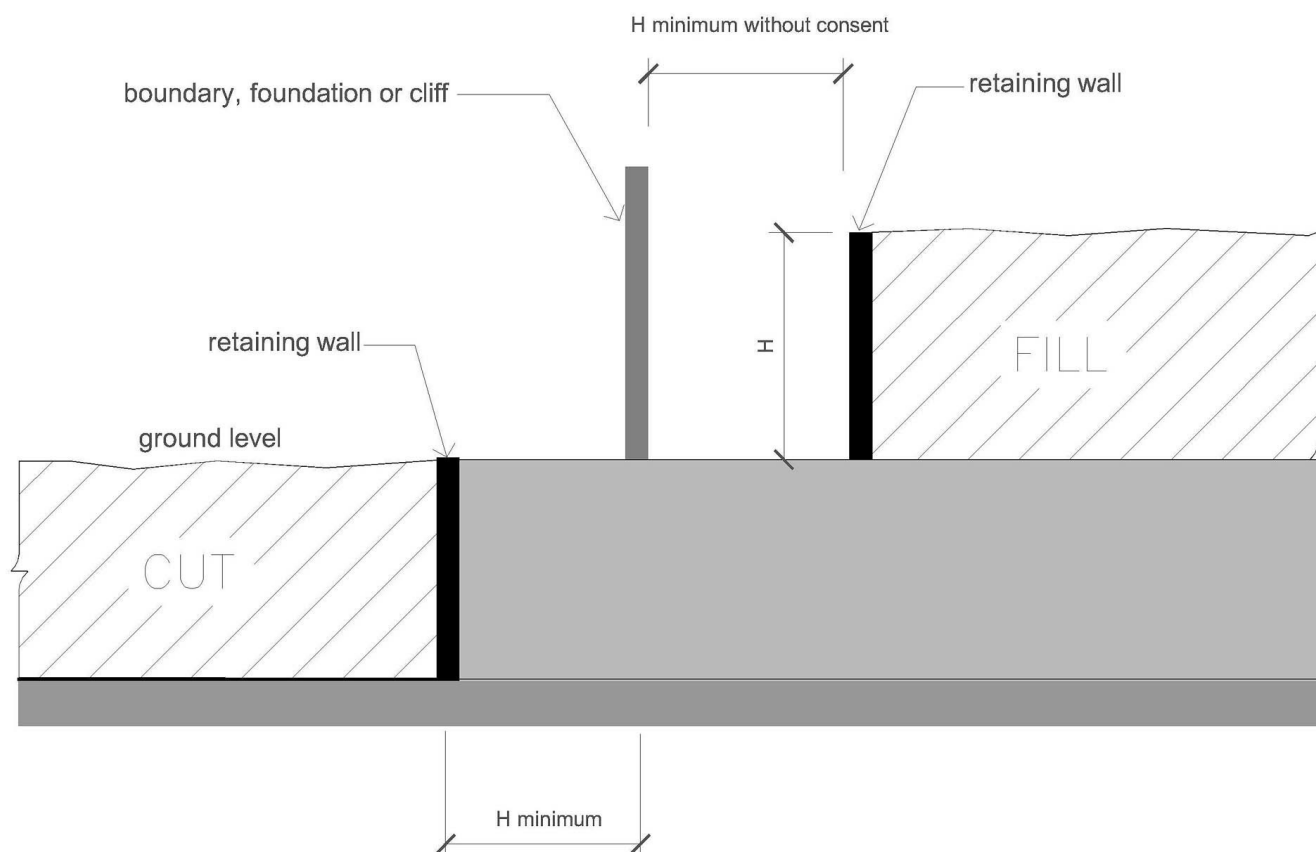
Earthworks over 600mm in height or depth must be set back from: property boundaries, foundations of buildings, structures greater than 10m<sup>2</sup>, and the top or toe of any cliff, the following minimum distances:

- a. Earthworks not supported by retaining walls:
  - i. a distance at least equal to the maximum height of the fill, as measured from the toe of the fill (see Figure 24.6A);
  - ii. a distance at least equal to 1.5 times the maximum depth of the cut, plus 300mm, as measured from the toe of the cut (see Figure 24.6A); and
  - iii. 300mm, as measured from the crest of any cut (see Figure 24.6A).
- b. Retaining walls supporting a cut or fill must be setback a distance at least equal to the height of the retaining walls (see Figure 24.6B), except:
  - i. retaining walls supporting a cut that have been granted building consent are exempt from this standard.
- c. Earthworks *ancillary to network utilities* activities and earthworks *ancillary to the operation, repair, and maintenance of the roading network* are exempt from the setback from property boundary, buildings, structures and cliffs performance standard.

**Figure 24.6A Unsupported cut and fill (elevation view)**



**Figure 24.6B Cut and fill supported by retaining walls (elevation view)**



#### 24.6.3.4 Setback from network utilities

Earthworks must comply with Rule 5.6.2.

#### 24.6.3.5 Sediment control

Earthworks must be undertaken in a way that prevents sediment entering water bodies, stormwater networks or going across property boundaries.

### 24.6.4 Fire Fighting

New residential buildings and subdivision activities must comply with Rule 9.3.3.

### 24.6.5 Hazardous Substances Quantity Limits and Storage Requirements

The storage and use of hazardous substances must comply with Rule 9.3.4.

### 24.6.6 Location and Screening of Service Areas

1. Service areas must be located or screened so that they are not visible at ground level from adjacent: residential activities, residential zoned properties, or publicly accessible places within or outside the zone.
2. Service areas must not encroach into required parking, loading or manoeuvring areas.

### 24.6.7 Maximum Height

1. The maximum height of new buildings and structures, and additions and alterations, must not exceed 20m above ground level.
2. Rooftop structures are exempt from the performance standard for height provided they do not exceed the maximum height limit for all other buildings and structures by more than 5m.

### 24.6.8 Minimum Floor Level

New buildings (including residential buildings) to be used for sensitive activities in the Hazard 2 (flood) Overlay Zone must comply with Rule 11.3.3.

### 24.6.9 Number, Location and Design of Ancillary Signs

#### 24.6.9.1 General

- a. Signs must comply with the following standards, except the following signs are exempt from these standards:
  - i. signs that are not visible from any public place outside the Dunedin International Airport Zone;
  - ii. 'regulatory' (requiring or prohibiting specified actions), 'warning' (informing of hazards or of other features requiring a safe response), or 'directional' (identifying the location of, or direction to destinations, routes, building entrances, and vehicle accesses) signs; and
  - iii. 'building names' (excluding sponsorship names).
- b. Signs located on or above the footpath must comply with Rule 6.7.2.
- c. Signs must comply with Rule 6.7.3 where visible from a road.
- d. Signs related to commercial activities, except service station activity, must not be visible from a public place outside the Dunedin International Airport Zone.
- e. Signs higher than 4m above ground level must only display the business name or major facility name.
- f. Signs other than those specified in Rules 24.6.9.2 and 24.6.9.3 are not allowed .

#### 24.6.9.2 Signs attached to buildings

- a. The height, above ground level, at the highest point of any sign, attached to a building is 8m.
- b. Signs must not be attached to roofs.
- c. Signs must not project higher than the lowest point of the roof, except where mounted against a parapet or gable end.
- d. Signs attached flat against a building must:
  - i. not cumulatively exceed 15% of the total wall area (excluding windows) that face the road frontage, or 8m<sup>2</sup>, whichever is lesser; and
  - ii. if attached to a verandah fascia, must not exceed a height of 500mm, or the height of a verandah fascia, whichever is greater.
- e. If attached on the underside of a verandah or protruding from a building façade, signs must:
  - i. where attached to the façade of a building, not protrude more than 1.5m from the façade;
  - ii. have a maximum area per display face of 2m<sup>2</sup>;
  - iii. have a maximum of 2 display faces per sign;
  - iv. not exceed 1 sign per 15m of road frontage; and
  - v. not exceed 3 signs per building, for buildings with a single premise.

#### 24.6.9.3 Freestanding signs

- a. The maximum number of freestanding signs is:
  - i. 1 per 50m of road frontage (including private airport roads) for permanently fixed freestanding signs; and
  - ii. 1 per 15m of road frontage (including private airport roads) for portable freestanding signs.
- b. Freestanding signs must not exceed:
  - i. 8m in height for permanently fixed freestanding signs;
  - ii. 4m in height for portable freestanding signs;
  - iii. an area of 16m<sup>2</sup> per display face for permanently fixed freestanding signs;
  - iv. an area of 8m<sup>2</sup> per display face for portable freestanding signs;
  - v. a width of 2m; and
  - vi. a depth of 400mm.
- c. Portable freestanding signs must be positioned entirely within site boundaries and not be located on the road reserve.
- d. Freestanding signs must not obstruct driveways, parking or loading areas.

#### **Note 24.6A - Other relevant District Plan provisions**

1. Commercial advertising is a non-complying land use activity in all zones except the Airport Zone.
2. See Section 3 Public Amenities for the rules related to public noticeboards
3. See Section 4 Temporary Activities for the rules related to temporary signs.

**Note 24.6B - Other requirements outside of the District Plan**

1. For additional restrictions that may apply to signs, see also:
  - a. New Zealand Transport Agency, Traffic Control Devices Manual, Part 3, Advertising Signs
  - b. Dunedin City Council Commercial Use of Footpaths Policy
  - c. Dunedin City Council Roadway Bylaw
  - d. Dunedin City Council Traffic and Parking Bylaw

**24.6.10 Parking, Loading and Access Standards**

Parking, loading and access must comply with Rule 6.6.

**Rule 24.7 Subdivision Performance Standards**

Subdivision activities must comply with performance standards for the Taieri Plains Rural Zone within Rule 17.7.



## **Rule 24.8 Assessment of Restricted Discretionary Activities (Performance Standard Contraventions)**

### **Rule 24.8.1 Introduction**

1. Restricted discretionary activities will be assessed in accordance with section 104 and 104C of the RMA, meaning only those matters to which Council has restricted its discretion will be considered, and Council may grant or refuse the application, and, if granted, may impose conditions with respect to matters over which it has restricted its discretion.
2. Rules 24.8.2 - 24.8.5:
  - a. list the matters Council will restrict its discretion to; and
  - b. provide guidance on how consent applications will be assessed, including:
    - i. relevant objectives and policies, with respect to s104(1)(b)(vi);
    - ii. potential circumstances that may support a consent application;
    - iii. general assessment guidance; and
    - iv. conditions that may be imposed.

### **24.8.2 Assessment of all performance standard contraventions**

| Performance standard                   | Guidance on the assessment of resource consents   |
|--|---|
| 1. Performance standard contraventions | <p><i>Potential circumstances that may support a consent application include:</i></p> <ol style="list-style-type: none"> <li>a. The degree of non-compliance with the performance standard is minor.</li> <li>b. The need to meet other performance standards or topography, or other site specific factors, make meeting the standard impracticable.</li> <li>c. Topography or other site specific factors make the standard irrelevant as the adverse effects that the standard is trying to manage will not occur.</li> <li>d. Non-compliance with a development performance standard would improve the design of the development in a way that would result in positive effects and better achieve the identified objectives and policies of the Plan.</li> </ol> <p><i>General assessment guidance:</i></p> <ol style="list-style-type: none"> <li>e. Where more than one standard is contravened, the combined effects of the contraventions should be considered.</li> </ol> |

### **24.8.3 Assessment of land use performance standard contraventions**

| Performance standard   | Matters of discretion  | Guidance on the assessment of resource consents   |
|--|--|---|
| 1. Location ( <i>commercial activities ancillary to airport activity</i> ) | a. Effects on the vibrancy, and economic and social success of the CBD and centres hierarchy | <p><i>Relevant objectives and policies:</i></p> <ol style="list-style-type: none"> <li>i. Objective 24.2.2</li> <li>ii. Commercial activity ancillary to airport activity is designed, location and operated to primarily service staff and visitors to the Dunedin International Airport (Policy 24.2.2.5).</li> </ol> |

#### 24.8.3 Assessment of land use performance standard contraventions

| Performance standard |                                      | Matters of discretion  | Guidance on the assessment of resource consents   |
|----------------------|--------------------------------------|--|---|
| 2.                   | Minimum car parking                  | a. Effects on the safety and efficiency of the transport network         | See Rule 6.9  |
|                      |                                      | b. Effects on accessibility  |   |
| 3.                   | Minimum vehicle loading              | a. Effects on the safety and efficiency of the transport network         | See Rule 6.9  |
| 4.                   | Number and location of tourism signs | a. Effects on rural amenity and amenity at Dunedin International Airport | <i>Relevant objectives and policies:</i> <ul style="list-style-type: none"> <li>i. Objective 24.2.1</li> <li>ii. The size, number and location of tourism advertising signage does not significantly affect the amenity of the airport as an international gateway, and any effects on the rural amenity outside the Dunedin International Airport Zone are minor (Policy 24.2.2.6).</li> </ul> |
| 5.                   | Service station standards            | a. Effects on the safety and efficiency of the transport network         | See Rule 6.9  |

#### 24.8.4 Assessment of development performance standard contraventions

| Performance standard |   | Matters of discretion         | Guidance on the assessment of resource consents  |
|----------------------|---|-------------------------------|--|
| 1.                   | Boundary setbacks                         | a. Effects on rural amenity   | <i>Relevant objectives and policies:</i> <ul style="list-style-type: none"> <li>i. Objective 24.2.2</li> <li>ii. Buildings and structures are of a setback that reduces their visual effects on the rural environment (Policy 24.2.2.1).</li> </ul>  |
| 2.                   | Boundary treatments and other landscaping | a. Effects on on-site amenity | <i>Relevant objectives and policies:</i> <ul style="list-style-type: none"> <li>i. Objective 24.2.2</li> <li>ii. Landscaping of new buildings, parking areas, and outdoor storage areas ensures a high standard of visual amenity is maintained adjacent to public roads and the rural zones (Policy 24.2.2.2).</li> </ul> |
|                      |   | b. Effects on rural amenity   |  |

#### 24.8.4 Assessment of development performance standard contraventions

| Performance standard |   | Matters of discretion   | Guidance on the assessment of resource consents  |
|----------------------|---|---|--|
| 3.                   | Earthworks standards:<br>• Batter gradient  | a. Effects on the stability of land, buildings and structures   | <p><i>Relevant objectives and policies:</i></p> <ul style="list-style-type: none"> <li>i. Objective 24.2.3</li> <li>ii. Earthworks and associated retaining structures are designed and located to avoid adverse effects on the stability of land, buildings, and structures by using a batter gradient that will be stable over time (Policy 24.2.3.1.b).</li> </ul> <p><i>Conditions that may be imposed include, but are not limited to:</i></p> <ul style="list-style-type: none"> <li>iii. Maximum slopes of cut and fill batters.</li> <li>iv. Time limits for retaining wall installation to avoid leaving a cut slope unsupported for an extended period.</li> <li>v. Temporary shoring requirements to maintain stability before a wall is constructed.</li> <li>vi. Supervision and monitoring requirements for retaining wall construction and standard earthworks construction.</li> </ul> |
| 4.                   | Earthworks standards:<br>• Setback from property boundary, buildings, structures and cliffs | a. Effects on the stability of land, buildings and structures   | <p><i>Relevant objectives and policies:</i></p> <ul style="list-style-type: none"> <li>i. Objective 24.2.3</li> <li>ii. Earthworks and associated retaining structures are designed and located to avoid adverse effects on the stability of land, buildings, and structures by being set back an adequate distance from property boundaries, buildings and cliffs (Policy 24.2.3.1.a).</li> </ul>   |
| 5.                   | Earthworks standards:<br>• Setback from network utilities                                   | a. Effects on efficient and effective operation of network utilities<br><br>b. Effects on health and safety | See Rule 5.7   |

#### 24.8.4 Assessment of development performance standard contraventions

| Performance standard |   | Matters of discretion   | Guidance on the assessment of resource consents   |
|----------------------|---|---|---|
| 6.                   | Earthworks standards:<br>• Sediment control                   | a. Effects on surrounding sites   | <i>Relevant objectives and policies:</i><br>i. Objective 24.2.3<br>ii. Earthworks and any associated retaining structures are designed and located to minimise adverse effects on surrounding sites and the wider area by managing earthworks to avoid sediment run-off, including on to any property, or into any stormwater pipes, drains, channels or soakage systems (Policy 24.2.3.2.b). |
|                      |   | b. Effects on biodiversity values and natural character of the coast and riparian margins | See Rule 10.4   |
|                      |   | c. Effects on the efficiency and/or affordability of infrastructure                       | See Rule 9.4  |
| 7.                   | Hazardous substances quantity limits and storage requirements | a. Effects on health and safety   | See Rule 9.4  |
|                      |   | b. Risk from natural hazards  | See Rule 11.4   |
| 8.                   | Location and screening of service areas                       | a. Effects on on-site amenity   | <i>Relevant objectives and policies:</i><br>i. Objective 24.2.2   |
|                      |   | b. Effects on rural amenity   | ii. Development maintains on-site and rural amenity by ensuring service areas are not visible from publicly accessible areas within or outside the zone (Policy 24.2.2.3).  |
| 9.                   | Maximum height  | a. Effects on rural amenity   | <i>Relevant objectives and policies:</i><br>i. Objective 24.2.2<br>ii. Buildings and structures are of a height that reduces their visual effect on the rural environment (Policy 24.2.2.1).  |
|                      |   | b. Effects on cultural values of manawhenua   | See Rule 14.3   |
| 10.                  | Minimum floor level (hazard overlays)                         | a. Risk from natural hazards  | See Rule 11.4   |

#### 24.8.4 Assessment of development performance standard contraventions

| Performance standard |  | Matters of discretion  | Guidance on the assessment of resource consents   |
|----------------------|--|--|---|
| 11.                  | Number, location and design of ancillary signs | a. Effects on rural amenity                                      | <i>Relevant objectives and policies:</i> <ol style="list-style-type: none"> <li>Objective 24.2.2</li> <li>Ancillary signs are located and designed to maintain visual amenity for airport users, including by being of an appropriate size and number to convey information about the name, location and nature of the activity on-site, and not being oversized or too numerous for that purpose (Policy 24.2.2.4).</li> </ol> |
|                      |  | b. Effects on safety and efficiency of the transport network     | See Rule 6.9  |
| 12.                  | Parking, loading and access standards          | a. Effects on the safety and efficiency of the transport network | See Rule 6.9  |

#### 24.8.5 Assessment of subdivision performance standard contraventions

| Performance standard |                                   | Guidance on the assessment of resource consents |
|----------------------|-----------------------------------|---|
| 1.                   | Subdivision performance standards | See Rule 16.9                                   |

## Rule 24.9 Assessment of Restricted Discretionary Activities

### Rule 24.9.1 Introduction

1. Restricted discretionary activities will be assessed in accordance with section 104 and 104C of the RMA, meaning only those matters to which Council has restricted its discretion will be considered, and Council may grant or refuse the application, and, if granted, may impose conditions with respect to matters over which it has restricted its discretion.
2. Rules 24.9.2 - 24.9.4:
  - a. list the matters Council will restrict its discretion to; and
  - b. provide guidance on how a consent application will be assessed, including:
    - i. relevant objectives and policies, with respect to s104(1)(b)(vi);
    - ii. potential circumstances that may support a consent application;
    - iii. general assessment guidance; and
    - iv. conditions that may be imposed.
3. For all land use activities that require consent, all associated development activities will be considered as part of the resource consent even if the development otherwise meets the development performance standards in the Plan. Conditions on development activities may be used to minimise any adverse effects from the land use activity or create mitigating positive effects.
4. Where a restricted discretionary activity does not meet a performance standard the following occurs:
  - a. if the contravention of the performance standard defaults to **restricted discretionary** (which is the case, unless otherwise indicated in the performance standard) then:
    - i. the activity, as a whole, will be treated as **restricted discretionary**; and
    - ii. the matters of discretion are expanded to include the areas of non-compliance with the performance standard; and
    - iii. the performance standard contravention will be assessed as indicated in Section 24.8; and
    - iv. the matters of discretion in this section will be assessed as indicated.
  - b. if the contravention of the performance standard defaults to **discretionary** then:
    - i. the activity, as a whole, will be treated as **discretionary**; and
    - ii. the performance standard contravention will be assessed as indicated in Section 24.10; and
    - iii. the assessment guidance in this section will also be considered.
  - c. if the contravention of the performance standard defaults to **non-complying** then:
    - i. the activity, as a whole, will be **non-complying**; and
    - ii. the performance standard contravention will be assessed as indicated in Section 24.11; and
    - iii. the assessment guidance in this section will also be considered.

#### 24.9.2 Assessment of restricted discretionary land use activities

| Activity            | Matters of discretion                                    | Guidance on the assessment of resource consents |
|---------------------|--|---|
| 1. Service stations | a. Effects on safety and efficiency of transport network | See Rule 6.10                                   |

#### 24.9.3 Assessment of all restricted discretionary development activities

| Activity  | Matters of discretion  | Guidance on the assessment of resource consents  |
|---|--|--|
| 1. Earthworks - large scale (that exceed the scale thresholds for the Dunedin International Airport Zone) | <p>a. Effects on visual amenity and character</p> <p>b. Effects on the amenity of surrounding properties</p> | <p><i>Relevant objectives and policies:</i></p> <ul style="list-style-type: none"> <li>i. Objective 24.2.3</li> <li>ii. Adverse effects on visual amenity and character will be avoided or, if avoidance is not possible, adequately mitigated (Policy 24.2.3.3.a).</li> <li>iii. Adverse effects on the amenity of surrounding properties, including from changes to drainage patterns, will be avoided or, if avoidance is not possible, adequately mitigated (Policy 24.2.3.3.b).</li> </ul> <p><i>Potential circumstances that may support a consent application include:</i></p> <ul style="list-style-type: none"> <li>iv. There is no, or only minimal, alteration to the natural landform.</li> <li>v. Any cut or fill will be restored or treated to resemble natural landforms.</li> <li>vi. The earthworks will not remove or effect existing vegetation or landscaping.</li> </ul> <p><i>Conditions that may be imposed include, but are not limited to:</i></p> <ul style="list-style-type: none"> <li>vii. Measures to minimise visual effects, e.g. requirements for revegetation and/or landscaping.</li> <li>viii. Maximum slopes of cut and fill batters.</li> <li>ix. Measures to divert surface water and rain away from, or prevent from discharging over, batter faces and other areas of bare earth.</li> <li>x. Measures to ensure there are no adverse effects from changes to drainage patterns on surrounding properties.</li> <li>xi. Requirement to de-compact soils; to take other remedial action to ensure the natural absorption capacity of the soils is not reduced; or to use other mitigation measures to ensure the overall absorption of rain water on-site is not diminished.</li> </ul> |
|   | c. Effects on the stability of land, buildings, and structures   | <p><i>Relevant objectives and policies:</i></p> <ul style="list-style-type: none"> <li>i. Objective 24.2.3</li> <li>ii. Adverse effects on the stability of land, buildings, and structures will be avoided or, if avoidance is not possible, adequately mitigated (Policy 24.2.3.3.c).</li> </ul> <p><i>Potential circumstances that may support a consent application include:</i></p> <ul style="list-style-type: none"> <li>iii. A geotechnical report confirms the existing ground is suitably stable for the proposed works, and proposed works will not create instability risks for surrounding land, buildings, or structures (see Special Information Requirements - Rule 24.12.1).</li> <li>iv. Excavation, fill and retaining structures will be designed, and the work undertaken, in accordance with best practice engineering standards</li> </ul>  |



### 24.9.3 Assessment of all restricted discretionary development activities

| Activity  | Matters of discretion                                    | Guidance on the assessment of resource consents |
|---|--|---|
| 2. Earthworks - large scale (that exceed scale thresholds for a hazard (flood) overlay zone)  | a. Risk from natural hazards                             | See Rule 11.5                                   |
| 3. In the <b>hazard 2 overlay zones</b> (see Rule 24.3.6):<br>• New buildings, and additions and alterations to buildings, which create more than 60m <sup>2</sup> of new ground floor area | a. Risk from natural hazards                             | See Rule 11.5                                   |
| 4. Parking areas which creates 50 or more new parking spaces  | a. Effects on safety and efficiency of transport network | See Rule 6.10                                   |
| 5. Service stations   | a. Effects on safety and efficiency of transport network | See Rule 6.10                                   |
|   | b. Effects on accessibility                              |   |

### 24.9.4 Assessment of restricted discretionary subdivision activities

| Activity                      | Matters of discretion  | Guidance on the assessment of resource consents   |
|-------------------------------|--|---|
| 1. All subdivision activities | a. Effects on the efficient and effective operation of Dunedin International Airport | <i>Relevant objectives and policies:</i><br>i. Objective 24.2.1<br>ii. Subdivision activities do not adversely affect the efficient and effective operation of Dunedin International Airport (Policy 24.2.1.5.b). |
|                               | See Rule 16.9  |   |



## **Rule 24.10 Assessment of Discretionary Activities**

### **Rule 24.10.1 Introduction**

1. Discretionary activities will be assessed in accordance with section 104 and 104B of the RMA meaning Council may grant or refuse the application, and, if granted, may impose conditions.
2. Rules 24.10.2 - 24.10.4 provide guidance on how a consent application for the listed discretionary activities will be assessed, including:
  - a. relevant objectives and policies that will be considered as a priority with respect to s104(1)(b)(vi);
  - b. potential circumstances that may support a consent applications;
  - c. general assessment guidance, including any effects that will be considered as a priority; and
  - d. conditions that may be imposed.
3. For all land use activities that require consent, all associated development activities will be considered as part of the resource consent even if the development otherwise meets the development performance standards in this Plan. Conditions on development activities may be used to minimise any adverse effects from the land use activity or create mitigating positive effects.

## 24.10.2 Assessment of all discretionary activities

| Activity   | Guidance on the assessment of resource consents  |
|--|--|
| <p>1. All discretionary activities, including:</p> <ul style="list-style-type: none"> <li>• office</li> <li>• all activities in the industrial activities category</li> <li>• activities listed below</li> </ul> | <p><i>Relevant objectives and policies (priority considerations):</i></p> <ul style="list-style-type: none"> <li>a. Objective 2.3.1</li> <li>b. Objective 24.2.1</li> <li>c. Activities that are ancillary to Dunedin International Airport activity are operated in line with Objective 22.4.2.2 and its policies (Policy 24.2.1.2).</li> <li>d. Activities that are not ancillary to Dunedin International Airport activity: <ul style="list-style-type: none"> <li>i. are related to or support the Dunedin International Airport, or have other operational requirements that mean they need to locate in the zone;</li> <li>ii. support the efficient and effective operation of Dunedin International Airport;</li> <li>iii. are not more appropriately located in another zone in line with Objective 2.3.2 and its policies; and</li> <li>iv. are designed and operated in line with Objective 24.2.2 and its policies (Policy 24.2.1.3).</li> </ul> </li> </ul> <p><i>Potential circumstances that may support a consent application include:</i></p> <ul style="list-style-type: none"> <li>e. For discretionary land use activities, whether any associated development activities meet relevant development performance standards, or are otherwise consistent with relevant objectives and policies for development.</li> <li>f. All relevant land use performance standards are met, including noise and light spill standards.</li> </ul> <p><i>General assessment guidance:</i></p> <ul style="list-style-type: none"> <li>g. In assessing the significance of effects, consideration will be given to: <ul style="list-style-type: none"> <li>i. short to long term effects, including effects in combination with other activities; and</li> <li>ii. the potential for cumulative adverse effects arising from similar activities occurring as a result of a precedent being set by the granting of a resource consent.</li> </ul> </li> <li>h. In assessing activities that are discretionary due to being in an overlay zone, mapped area, in a scheduled site, or affecting a scheduled item, that otherwise require resource consent, the assessment guidance provided in relation to the underlying activity status will also be considered.</li> </ul> |

#### 24.10.3 Assessment of discretionary land use activities

| Activity  | Guidance on the assessment of resource consents  |
|---|--|
| 1. In the <b>hazard 2 overlay zones</b> (see Rule 24.3.6): <ul style="list-style-type: none"> <li>• Potentially sensitive activities not permitted in Dunedin International Airport Zone</li> <li>• Sensitive activities</li> </ul> | See Section 11.6 for guidance on the assessment of resource consents in relation to Objective 11.2.1 and effects related to the risk from natural hazards. |

#### 24.10.4 Assessment of discretionary performance standard contraventions

| Performance standard  | Guidance on the assessment of resource consents   |
|---|---|
| 1. <ul style="list-style-type: none"> <li>• Acoustic insulation</li> <li>• Noise - where the limit is exceeded by up to 5dB LAeq (15min)</li> <li>• Light spill - where the limit is exceeded by 25% or less</li> </ul> | See Section 9.6 for guidance on the assessment of resource consents in relation to Objective 9.2.2 and effects related to public health and safety. |

## Rule 24.11 Assessment of Non-complying Activities

### Rule 24.11.1 Introduction

1. Non-complying activities will be assessed in accordance with section 104, 104B and 104D of the RMA meaning Council may grant or refuse the application, and, if granted, may impose conditions.
2. Rules 24.11.2 - 24.11.4 provide guidance on how a consent application for the listed non-complying activities will be assessed, including:
  - a. relevant objectives and policies that will be considered as a priority with respect to s104(1)(b)(vi); and
  - b. general assessment guidance, including any effects that will be considered as a priority.
3. For all land use activities that require consent, all associated development activities will be considered as part of the resource consent even if the development otherwise meets the development performance standards in this Plan. Conditions on development activities may be used to minimise any adverse effects from the land use activity or create mitigating positive effects.

#### 24.11.2 Assessment of all non-complying activities

| Activity                        | Guidance on the assessment of resource consents   |
|---------------------------------|---|
| 1. All non-complying activities | <p><i>Relevant objectives and policies (priority considerations):</i></p> <ol style="list-style-type: none"> <li>a. Objective 2.3.1, Policy 2.3.1.1</li> <li>b. Objective 24.2.1, Policy 24.2.1.3</li> </ol> <p><i>General assessment guidance:</i></p> <ol style="list-style-type: none"> <li>c. In assessing the significance of effects consideration will be given to:               <ol style="list-style-type: none"> <li>i. both short and long term effects, including effects in combination with other activities; and</li> <li>ii. the potential for cumulative adverse effects arising from similar activities occurring as a result of a precedent being set by the granting of a resource consent.</li> </ol> </li> <li>d. In assessing activities that are non-complying due to being in an overlay zone, mapped area, in a scheduled site, or affecting a scheduled item, that otherwise require resource consent, the assessment guidance provided in relation to the underlying activity status will also be considered.</li> </ol> |

#### 24.11.3 Assessment of non-complying land use activities

| Activity  | Guidance on the assessment of resource consents   |
|---|---|
| 1. Commercial advertising ( <i>except tourism advertising</i> ) | <p><i>Relevant objectives and policies (priority considerations):</i></p> <ol style="list-style-type: none"> <li>a. Objective 2.4.1</li> <li>b. Policy 2.4.1.6.c</li> </ol> |

#### 24.11.4 Assessment of non-complying performance standard contraventions

| Performance standard    | Guidance on the assessment of resource consents  |
|-------------------------|--|
| 1. Archaeological sites | See Section 13.8 for guidance on the assessment of resource consents in relation to Objective 13.2.4 and effects related to the inappropriate development and use in Dunedin's archaeological sites. |

#### 24.11.4 Assessment of non-complying performance standard contraventions

| Performance standard  | Guidance on the assessment of resource consents  |
|---|--|
| <p>2.</p> <ul style="list-style-type: none"> <li>• Light spill - where the limit is exceeded by greater than 25%</li> <li>• Noise - where the limit is exceeded by 5dB LAeq (15 min) or more</li> <li>• Hazardous substances quantity limits and storage requirements (Rule 9.3.4.2)</li> </ul> | <p>See Section 9.7 for guidance on the assessment of resource consents in relation to Objective 9.2.2 and effects related to public health and safety.</p> |

### Rule 24.12 Special Information Requirements

#### 24.12.1 Geotechnical investigation report

1. A geotechnical investigation report may be requested by Council for earthworks of a large scale and/or where the earthworks are proposed:
  - a. on a site with a slope angle between 15° (3.7h:1v slope ratio, or 27% slope grade) and 26° (2h:1v slope ratio, or 50% slope grade);
  - b. on a site identified as hazard-prone in Council's Hazard Information Management System; or
  - c. on any other site that the Council, with good cause, suspects to be hazard-prone.
2. A geotechnical investigation report must be provided for earthworks on all sites with a slope greater than a 26° angle (2h:1v slope ratio, or 50% slope grade).
3. All requested geotechnical investigation reports must be prepared by a suitably qualified expert who is experienced in the practice of geotechnical engineering and registered under the Chartered Professional Engineers Act of New Zealand 2002 and who has professional indemnity insurance.
4. The geotechnical investigation report must address the following factors:
  - a. special design or construction requirements;
  - b. special foundation requirements;
  - c. services;
  - d. access;
  - e. effluent disposal;
  - f. non-engineered fills; and
  - g. a statement of professional opinion as to the suitability of the land for the proposed development.